

Central London Cycle Grid

Cycle Route Link in Bayswater

Public Consultation Report (Stage 1 Feasibility)

This report summarises public consultation undertaken during design development (Stage 1 Feasibility) of proposals to improve cycling along Hereford Road and Talbot Road, developed as part of the Central London Cycle Grid.

Background

Westminster City Council, in partnership with Transport for London and other local authorities, is supporting the delivery of the proposed Central London Cycle Grid, which comprises Quietways and Cycle Superhighways.

The Cycle Route Link in Bayswater seeks to improve the provision for cycling along quieter streets, particularly for people wishing to avoid some of the busier main roads in the area.

The section of this route being consulted on within the City of Westminster is approximately 1km in length. The streets affected by these proposals are Prince's Square, Leinster Square, Hereford Road and Talbot Road.

The Cycle Route Link in Bayswater will intersect with the proposed Quietway route from Bayswater Road to Edgware Road at Prince's Square. To the west of Ledbury Road, this proposed Cycle Route Link in Bayswater will extend into the Royal Borough of Kensington and Chelsea (RBKC),

As part of the assessment of the feasibility of this proposed Quietway cycling route, public consultation was undertaken in February 2016 and March 2016. Public consultation sought the views of residents, visitors, business owners and other interested groups to support the development and delivery of the Central London Cycle Grid. As Quietways are intended to attract new, less confident and beginner cyclists to make short trips by bicycle, engagement was considered key to garnering interest and enthusiasm for the programme of projects, raising awareness, and ultimately, achieving longer term behavioural change.

Pre-public consultation

The pre-consultation phase included the following aspects:

- A Public Realm Advisory Group (PRAG) meeting in Westminster City Council
- A Parking Review Group (PRG) meeting in Westminster City Council
- A pre-consultation meeting, inviting key stakeholders to discuss key issues along the route, including Councillors, local Amenity Societies, adjacent managing authorities, Living Streets, London Cycling Campaign, and CTC
- A Design Review by the Sponsor team in Transport for London

Public consultation overview

Public consultation started on 18th February 2016 and ended on 18th March 2016.

The section of the proposed Central London Cycle Grid that was consulted on is approximately 1km in length and is due for completion in 2016, subject to the outcome of the consultation.

The findings of the consultation will help shape the design proposals for this section of the Central London Cycle Grid at the next stage of design (stage 2). Proposals presented during public consultation (stage 1 feasibility design drawings) are shown in Appendix A.

Approach to consultation

Several different approaches were used during public consultation to raise awareness of the Central London Cycle Grid and this Quietway cycling route, in order try to gain a wide range of views and responses. The following methods were used:

- **Letters were sent to stakeholders within a 100m radius along the route of the Quietway including residents, businesses and schools.** The letter is shown in Appendix B. Approximately 3,700 letters were posted. The letter distribution area is shown in Appendix C. Authored by Councillor Heather Acton, Cabinet

Member for Sustainability and Parking, the letter helped to explain the proposed specific interventions along the proposed cycle route and their likely impacts. The letter included the web address where design proposals could be seen and commented on. The letter also included information on how to request hard copy plans of proposals.

- **Letters were also emailed to key stakeholders** (including ward Councillors, landowners, adjacent managing authorities, Residents' Associations and schools). The list of stakeholders is shown in Appendix D.
- **Design proposals and a questionnaire were hosted online on Westminster City Council's website.** This included explanatory text and an interactive map of the Quietway route being consulted on. There was an online form (i.e. a questionnaire) to capture comments and responses. The questionnaire included a free form response box to capture as many opinions as possible. **38 people accessed the online questionnaire - of these, 37 completed the questionnaire.** Only completed questionnaires were retained for analysis. The questionnaire is shown in Appendix E. To help understand opinions, the route was divided into 2 sections:
 - Prince's Square, Leinster Square and Hereford Road
 - Talbot Road
- **Public exhibitions were held on the 2nd March 2016 and 12th March 2016 at Paddington Library, 45 Porchester Road, W2 5DU.** This provided an opportunity for members of the public to view proposals, and to discuss them with the design team. A questionnaire was provided (consistent with the online form) to capture views. **Around 20 people attended these events.**
- Responses were encouraged through the online questionnaire. In addition, an email address and a telephone number were provided to allow respondents to share their views with the design team. **6 emails were received** (Appendix F).
- Westminster City Council's Policy, Performance and Communications team issued press releases and used social media to encourage awareness of the consultation.

Findings

- Respondents indicated that they principally found out about the proposals by receiving a letter from Westminster City Council. This response accounted for 36% of the answers. It should be noted that only 37 people responded to the consultation.
- **Overall, the respondents expressed support towards the proposals. Along the length of the route:**
 - **71% of respondents stated that they "strongly support" or "tend to support" the proposals.**
 - **8% of respondents stated that they "support some elements but not all".**
 - **4% of respondents stated that they "neither support nor oppose" the proposals.**
 - **16% of respondents stated that they "tend to oppose" or "strongly oppose" the proposals.**
- Along **Prince's Square, Leinster Square and Hereford Road**, 78% of respondents (29 people) stated that they "strongly support" or "tend to support" the proposals or "support some elements but not all". 16% stated that they "tend to oppose" or "strongly oppose" the proposals
- Along **Talbot Road**, 81% of respondents (30 people) stated that they "strongly support" or "tend to support" the proposals or "support some elements but not all". 16% stated that they "tend to oppose" or "strongly oppose" the proposals.

Data analysis and charts summarising this data are shown in Appendix G.

- Among respondents who entered a home postcode which is within the City of Westminster (24 people), referred to as **Residents** in Appendix G, there is **strong support** for the proposals. 71% stated that they "strongly support" or "tend to support" the proposals. A further 10% "support some elements but not all". 6% stated that they "neither support nor oppose" the proposals. 13% stated that they "tend to oppose" or "strongly oppose" the proposals.

- Among respondents who entered a home postcode which is outside of the City of Westminster (13 people), referred to as **Non-Residents** in Appendix G, 73% stated that they “strongly support” or “tend to support” the proposals. A further 4% “support some elements but not all”. 0% stated that they “neither support nor oppose” the proposals. 23% stated that they “tend to oppose” or “strongly oppose” the proposals.
- Among respondents who cycle every day or a few times a week (17 people), referred to as **Cyclists** in Appendix G, there is **strong support** for the proposals. 79% stated that they “strongly support” or “tend to support” the proposals. A further 12% “support some elements but not all”. 3% stated that they “neither support nor oppose” the proposals. 6% stated that they “tend to oppose” or “strongly oppose” the proposals.
- Among respondents who cycle once a week or less (20 people), referred to as **Non-Cyclists** in Appendix G, 65% stated that they “strongly support” or “tend to support” the proposals. A further 5% “support some elements but not all”. 5% stated that they “neither support nor oppose” the proposals. 25% stated that they “tend to oppose” or “strongly oppose” the proposals.
- The majority of respondents stated that they “agree” or “strongly agree” that their enjoyment of central London and of the City of Westminster is affected by air quality (86%), overcrowded public transport systems (70%), traffic congestion (78%) and road traffic collisions (62%).
- The majority of respondents stated that they “agree” or “strongly agree” that more people cycling for everyday journeys can help to solve these issues. (81%,83%,86% and 69% respectively)
- 27% of the respondents stated that in the area being consulted on, the cycling conditions are currently “very good” or “fairly good”, 11% stated that they are “neither good nor poor” and 62% stated that they are “fairly poor” or “very poor”.
- 84% of respondents said they “strongly agree” or “agree” that they would be more likely to cycle in central London if there was a network of easy to follow, quiet cycle routes, whilst 8% stated that they “disagree” or “strongly disagree”.

Key Themes

A number of key themes were raised during consultation.

- Based on the online consultation feedback, the views expressed by respondents were **supportive** of the proposals for this Quietway. Overall, **79% of respondents support or partially support the proposals**. Among respondents who stated that their home address is within the City of Westminster (24 people), **81% support or partially support the proposals**.
- This is an **established cycle route**, with comparatively low volumes of traffic. Some respondents have called for **traffic calming** measures on Hereford Road and Talbot Road, stating that there is a perceived **speeding and rat-running** issue on Hereford Road.
- There are concerns about the **width of Hereford Road** and a potential **shortage of passing places**. Generally, respondents wish to maintain two-way operation in Hereford Road.
- There are requests to review **the junction of Westbourne Grove and Hereford Road**, to help cyclists and pedestrians.
- There is a request to increase the **Santander cycle hire availability** in the St Stephen’s area of W2.
- There is a request to add a pedestrian phase to the **junction of Talbot Road and Chepstow Road**.
- There are general concerns about **sharing space between pedestrians and cyclists**. However, there are no such instances of shared space in the proposals for this Quietway. The design team continues to place cyclists on the carriageway as per best practice wherever feasible.
- There are concerns regarding a narrow contra-flow cycle lane in the Royal Borough of Kensington and Chelsea (RBKC) along Talbot Road; these concerns have already been addressed by the Royal Borough of Kensington and Chelsea Borough Council through implementation of improved contraflow cycling provision along this section of Talbot Road, as part of the Central London Cycle Grid.

Recommendations

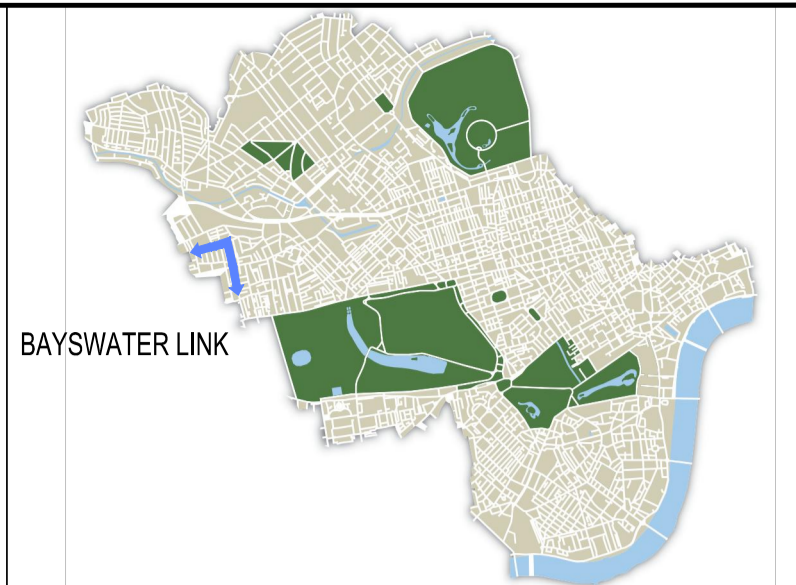
The proposals are well received, both by the overall respondents (79% support or partial support) and by the respondents who stated that their home address is within the City of Westminster (81% support or partial support). Based on these results, and the comments received, we believe that there is broad consensus for the level of intervention proposed. Based on the outcome of consultation, it is recommended to consider key issues and themes

raised during consultation to help inform the decision by Westminster City Council to continue the design and implementation of this proposed Quietway.

Based on the results of the consultation, the following recommendations should be considered:

- It is recommended that the design and construction of this proposed cycle route link in Bayswater is incorporated into the design and construction of the proposed Quietway from Bayswater to Edgware Road.
- Consider the feasibility of introducing additional measures at the junction of Hereford Road and Westbourne Grove, to help pedestrians and cyclists. Potential improvement measures may include signalisation of the junction.
- Consider the feasibility of introducing traffic calming measures along Hereford Road and Talbot Road.
- Subject to TfL funding, additional cycle parking facilities should be investigated along the route to accommodate additional cycle journeys. Moreover, an extension to the cycle hire scheme area should be investigated.

Appendix A – Proposals presented during public consultation



BAYSWATER LINK

- KEY**
- PROPOSED WHITE LINE MARKING
 - PROPOSED CYCLE SYMBOL (WAYFINDING MARKER TO BE CONFIRMED)
 - EXISTING DIRECTION OF TRAVEL
 - EXISTING KERB
 - EXISTING LINE MARKING
 - ADJOINING CYCLE ROUTE
 - BOROUGH BOUNDARY
 - ROUTE ALIGNMENT

NOTES

1. Design development is ongoing and subject to change following consultation.
2. Design of Advanced Stop Line to be revised following publication of Traffic Signs Regulations and General Directions (TSRGD), expected 2016

REV	DATE	BY	DESCRIPTION	CHK	APP
A	17.02.2016	CEP	FIRST ISSUE	RH	NW

CONSULTATION

25 Mandela Way, London, SE1 5SZ
www.fonway.co.uk www.wspgroup.com

CLIENT

City of Westminster

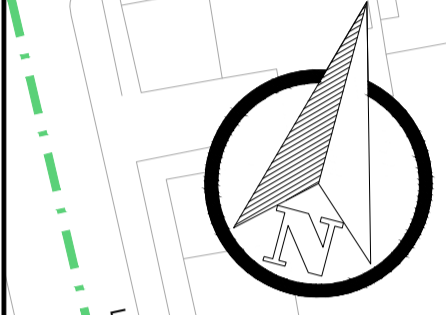
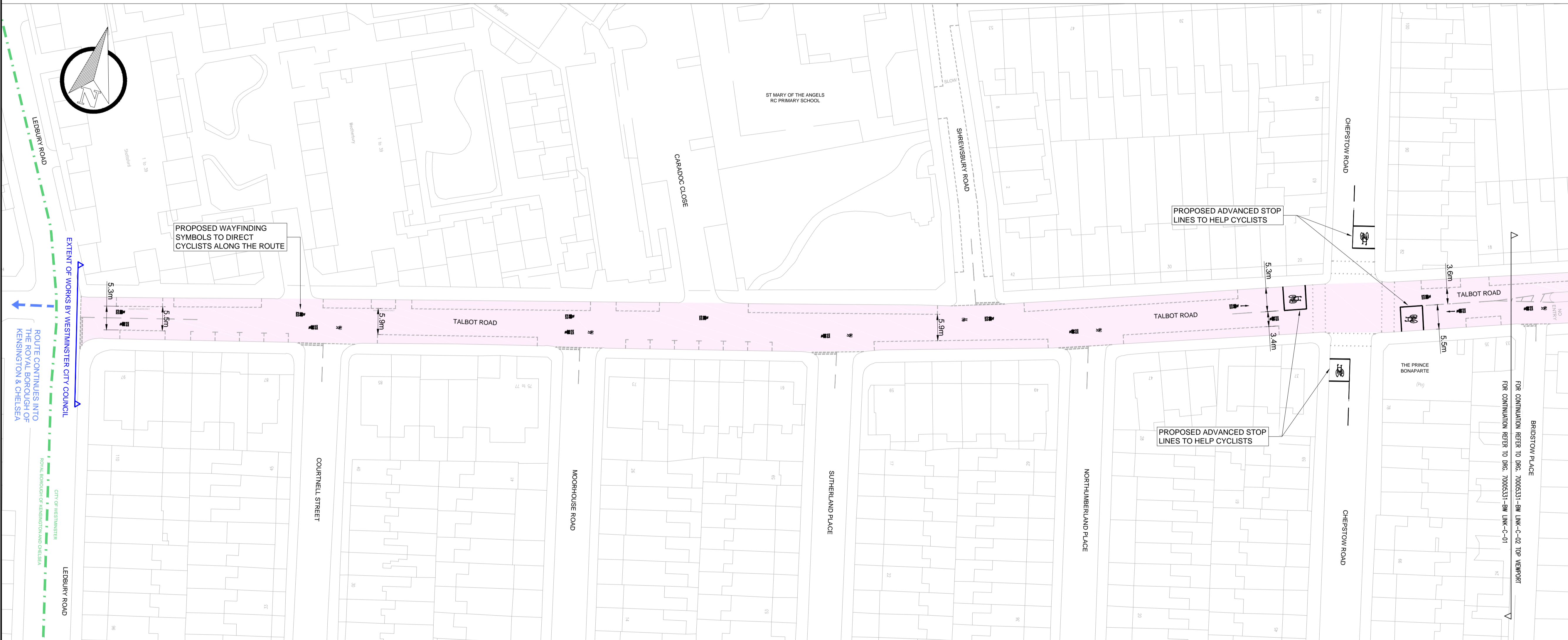
PROJECT

CENTRAL LONDON CYCLE GRID
BAYSWATER LINK

TITLE

TALBOT ROAD TO PRINCE'S SQUARE
WITHIN THE CITY OF WESTMINSTER
SHEET 1 OF 2

SCALE @ A1	DATE	DESIGN/DRAWN	CHECKED	APPROVED	SHEET
1:500	29/01/16	CEP	RH	NW	A1
JOB No:	FMC PROJECT No:	BREF/DSS No:			
70005331	WA1505JV4	BR 21505			
DRAWING No:					REV.
70005331-BW LINK-C-01					A



PROPOSED WAYFINDING SYMBOLS TO DIRECT CYCLISTS ALONG THE ROUTE

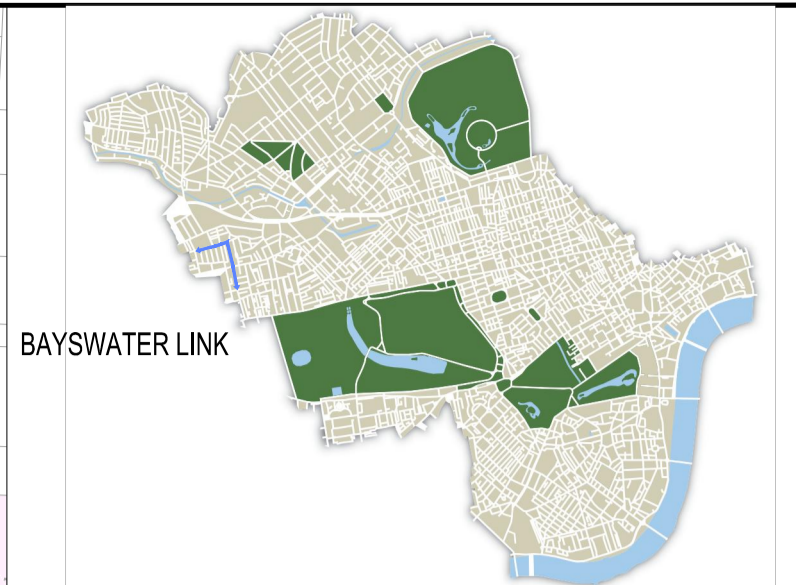
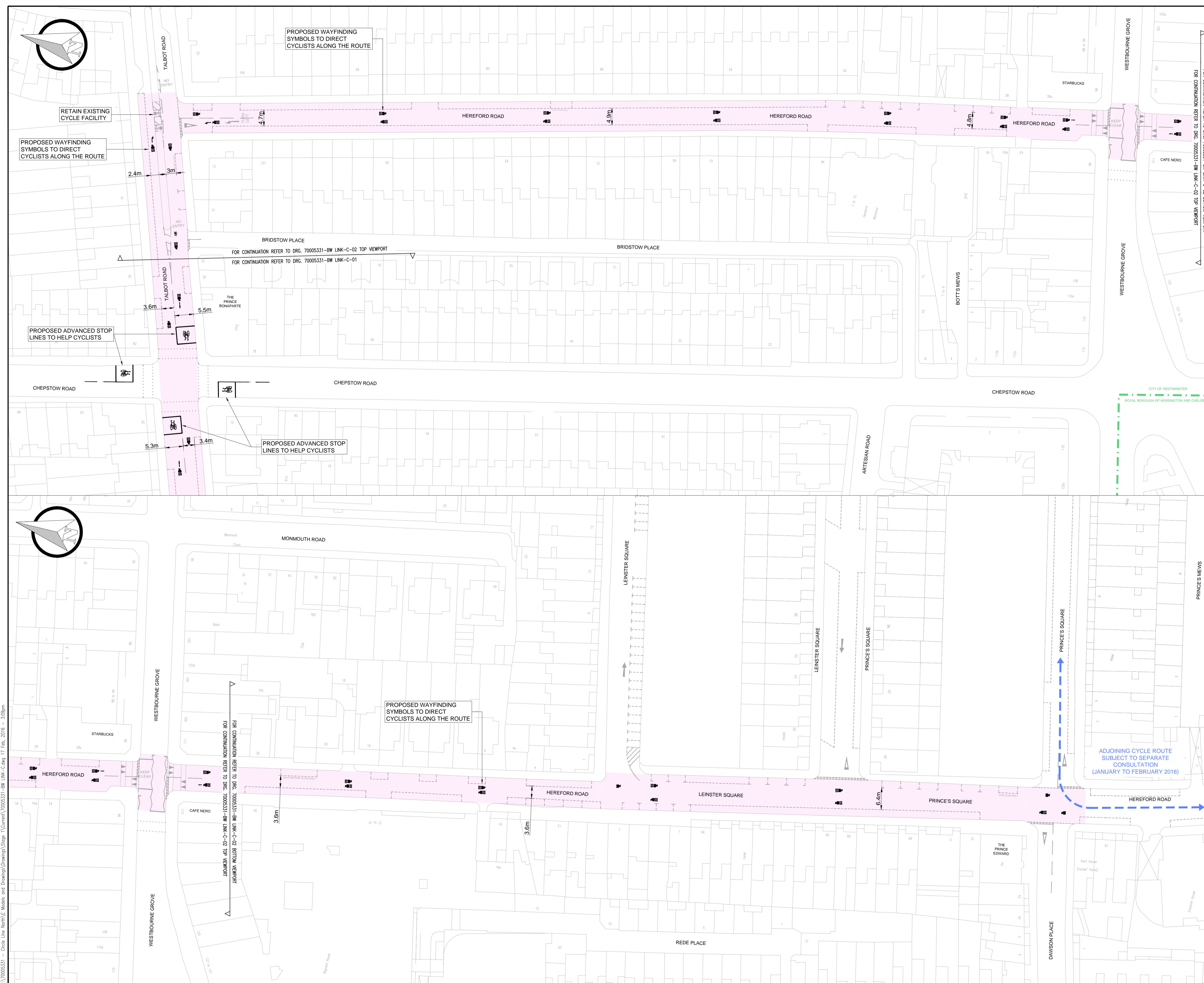
PROPOSED ADVANCED STOP LINES TO HELP CYCLISTS

PROPOSED ADVANCED STOP LINES TO HELP CYCLISTS

EXTENT OF WORKS BY WESTMINSTER CITY COUNCIL

ROUTE CONTINUES INTO THE ROYAL BOROUGH OF KENSINGTON & CHELSEA

T:\20005331 - Cycle Line North\1 - Models and Drawings\Drawings\Stage 1\Content\70005331-BW LINK-C-01.dwg 17 Feb 2016 - 3:08pm



- KEY**
- PROPOSED WHITE LINE MARKING
 - PROPOSED CYCLE SYMBOL (WAYFINDING MARKER TO BE CONFIRMED)
 - EXISTING DIRECTION OF TRAVEL
 - EXISTING KERB
 - EXISTING LINE MARKING
 - ADJOINING CYCLE ROUTE
 - - - BOROUGH BOUNDARY
 - █ ROUTE ALIGNMENT

- NOTES**
- Design development is ongoing and subject to change following consultation.
 - Design of Advanced Stop Line to be revised following publication of Traffic Signs Regulations and General Directions (TSRGD), expected 2016

REV	DATE	BY	DESCRIPTION	CHK	APP
A	17.02.2016	CEP	FIRST ISSUE	RH	NW

DRAWING STATUS: **CONSULTATION**



PROJECT: **CENTRAL LONDON CYCLE GRID
BAYSWATER LINK**

TITLE: **TALBOT ROAD TO PRINCE'S SQUARE
WITHIN THE CITY OF WESTMINSTER**

SHEET 2 OF 2

SCALE @ A1: 1:500	DATE: 29/01/16	DESIGN/DRAWN: CEP	CHECKED: RH	APPROVED: NW	SHEET: A1
JOB No: 70005331	FMC PROJECT No: WA1505JV4	BREF/DSS No: BR 21505	DRAWING No: 70005331-BW LINK-C-02		
					REV: A

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Appendix B – Letter



Contact: cyclegrid@westminster.gov.uk
Phone: 020 7641 1109
Ref: CLCG_CLNWa
Date: 16 February 2016

Dear Sir / Madam,

Consultation on the Central London Cycle Grid, Cycle Route link in Bayswater

Westminster City Council, in partnership with Transport for London and other local authorities, is supporting the delivery of a proposed Central London Cycle Grid. This Cycle Grid is being funded under the Mayor of London's Vision for Cycling, a 10-year plan delivering cycling improvements across London. The plan includes a network of Quietways and Cycle Superhighway routes, providing connected ways for cycling across central London.

This project aims to improve provision for cycling on streets along a proposed Quietway cycle link in Bayswater. It will benefit all people who want to cycle in the area, particularly those wishing to avoid some of the busier, highly trafficked main roads.

The section of the Central London Cycle Grid being consulted on is approximately 1km in length within the City of Westminster and will continue into the Royal Borough of Kensington and Chelsea along Talbot Road, west of Ledbury Road. The Streets affected by this proposal are Prince's Square, Leinster Square, Hereford Road and Talbot Road.

It is intended that this short cycling route will connect to a proposed Quietway cycle route between Bayswater Road and Edgware Road, which is currently being consulted on by Westminster City Council until 11th March 2016, and can be viewed at <https://www.westminster.gov.uk/quietway-route-bayswater-road-edgware-road>

Proposed intervention measures

Along **Hereford Road**, including the western arm of **Prince's Square** and the western arm of **Leinster Square**, we are proposing new wayfinding symbols on the road to help cyclists follow this route.

Along **Talbot Road**, wayfinding symbols are also proposed on the road to help cyclists follow this route. We are proposing Advanced Stop Line facilities at the junction of **Talbot Road** and **Chepstow Road** to assist cyclists.

Please tell us what you think

We would be grateful if you would visit our online consultation at <https://www.westminster.gov.uk/cycling-consultations> to view the proposal plans for a cycle route link along Prince's Square, Leinster Square, Hereford Road and Talbot Road, and to share your views of these proposals with us. This consultation closes on Friday 18th March 2016.

If you would prefer to view paper copies of proposals, please request these using the contact details on the top of this letter. Please include the reference number CLCG_CLNWA when you contact us.

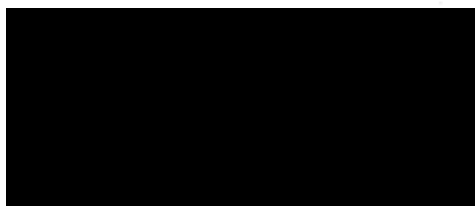
Public Exhibitions, where you will be able to ask questions and view plans, will be held at the Paddington Library, 45 Porchester Rd, W2 5DU on:

- Wednesday 2nd March 2016 (5pm-7pm)
- Saturday 12th March 2016 (10am-12pm)

Please visit our website to see more details on these public exhibitions.

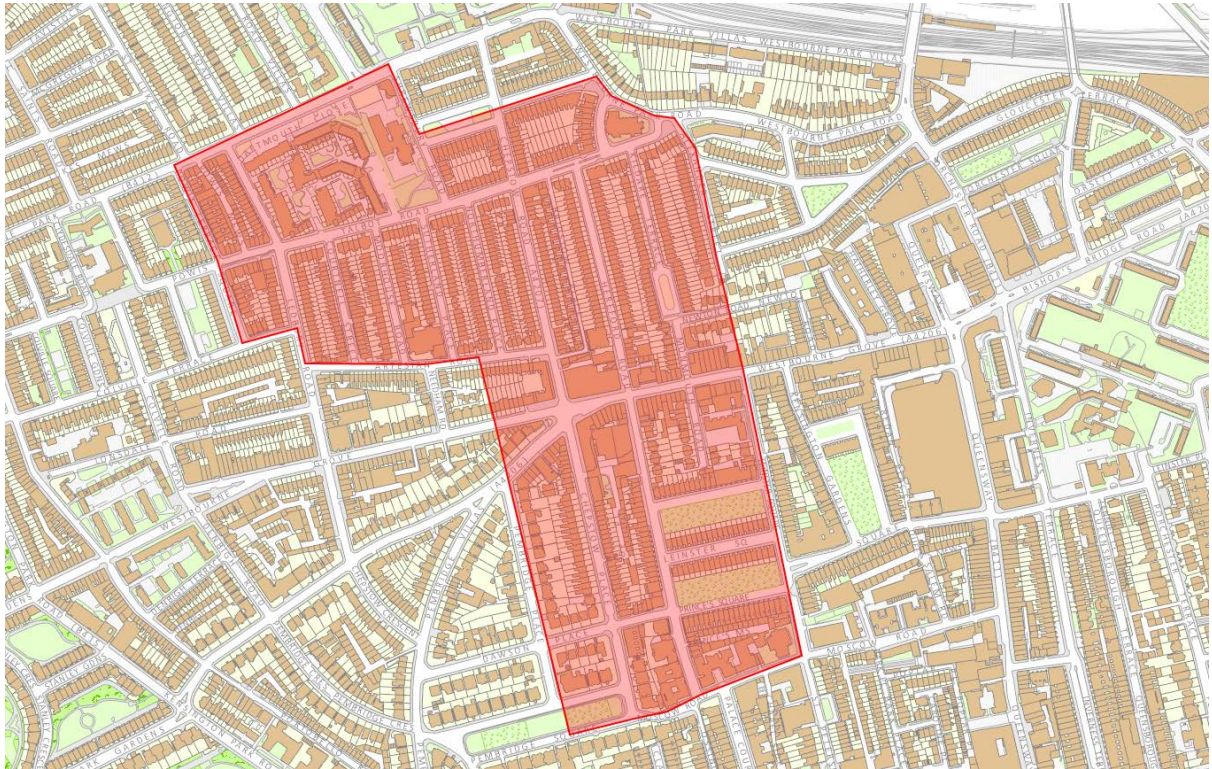
Yours faithfully,

Councillor Heather Acton



Cabinet Member for Sustainability and Parking

Appendix C – Extents of Letter Drop



Letter drop zone for the Cycle Route Link in Bayswater

A distance of approximately 100m on either side of the route alignment was defined by Westminster City Council for the letter drop area. The letter drop zone comprises approximately 3,700 addresses.

Appendix D – Key Stakeholders Contacted

	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London
	Transport for London (Buses)
	Transport for London (Buses)
	Transport for London Surface Transport Communications
	Transport for London
	Royal Borough of Kensington and Chelsea
	Royal Borough of Kensington and Chelsea
	Royal Borough of Kensington and Chelsea
	Royal Borough of Kensington and Chelsea
	Royal Borough of Kensington and Chelsea
	Royal Borough of Kensington and Chelsea
	Royal Borough of Kensington and Chelsea
	Royal Borough of Kensington and Chelsea
	North Paddington Society
	Wessex Gardens Residents' Association
	Westbourne Neighbourhood Association
	Westbourne Neighbourhood Association
	Westminster Residents' Action Group
	20 is Plenty Campaign
	British Medical Association
	British Telecom National Noticing Centre
	Cab Shelter Fund
	Cable and Wireless
	Confederation of Passenger Transport UK
	CTC
	FM Conway
	FM Conway
	FM Conway
	FM Conway
	Freight Transport Assoc. Ltd.
	Licensed Private Hire Car Association
	Living Streets
	Living Streets
	London Ambulance
	London Ambulance
	London Ambulance
	London Ambulance
	London Cab Drivers Club

	London Chamber of Commerce
	London Cycling Campaign
	London Cycling Campaign
	London Cycling Campaign
	London TravelWatch
	Metropolitan Police Service
	Metropolitan Police Service
	Metropolitan Police Service
	National Grid
	National Grid
	National Grid
	National Grid
	National Grid
	National Grid
	RMT London Taxi Drivers' Branch
	Royal Mail
	Taxi & Private Hire
	Thames Water Utilities
	The British Motorcyclists' Federation
	The Licensed Taxi Drivers' Association (LTDA)
	The London Fire Brigade
	The London Fire Brigade
	The London Fire Brigade
	The London Fire Brigade
	The London Fire Brigade
	The London Fire Brigade
	The London Fire Brigade
	The Road Haulage Assoc. Ltd.
	Transport for All
	Unite the Union (Cab Section)
	Vice Chair Westminster Liberal Democrats
	Weatherby Preparatory School
	Westminster Living Streets Group
	Westminster Living Streets Group
	Westminster Living Streets Group
	Westminster Living Streets Group
	Westminster Property Association
	WSP-Parsons Brinckerhoff
	WSP-Parsons Brinckerhoff
	WSP-Parsons Brinckerhoff
	WSP-Parsons Brinckerhoff
	WSP-Parsons Brinckerhoff
	Notting Hill East Neighbourhood Forum
	Westbourne Neighbourhood Forum
	College Park School
	Lansdowne College
	St Mary of the Angels RC Primary School
	City West Homes
	City West Homes
	City West Homes

	City West Homes
	London Forum of Amenity & Civic Societies Residents
	Westminster Residents Panel
	Ramada Jarvis Hyde Park
	Pembridge Hall School
	St Sophia Greek Cathedral
	Grand Plaza
	The Prince Edward (PH)
	Grand Plaza Bayswater Hotel
	Campaign for Better Transport
	The New Linden Hotel
	Wetherby School
	St Stephen's Church
	St Sophia Greek Cathedral
	The Prince Edward PH
	Ross Sloan Estate Agent
	Hereford Road Café
	Hafez Persian Cuisine
	Café Aphrodite
	Durbar
	Moroccan Sahara Restaurant
	Cote Brasserie
	Savana Urban Spa
	Commander Oyster Bar
	Churrascoria Bar and Grill
	Bonaparte Pub
	Wessex gardens Estate (City West Homes)
	Notting Hill Vet
	The Ledbury Restaurant
	All Square Group
	Nama
	Ming
	Natural Mat
	Royal Borough of Kensington and Chelsea Cycling Campaign

Appendix E – Questionnaire

Questionnaire for Consultation

Quietway Link Bayswater

Westminster City Council, in partnership with Transport for London and other local authorities, is supporting the delivery of a proposed Central London Cycle Grid. This Cycle Grid is being funded under the Mayor of London's Vision for Cycling, a 10-year plan delivering cycling improvements across London. The plan includes a network of Quietways and Cycle Superhighway routes, providing connected ways for cycling across central London.

The section of this route being consulted on within the City of Westminster is approximately 1km in length.

More information about the Westminster Cycle Strategy can be found at <https://www.westminster.gov.uk/cycling-consultations>

More information about the Central London Cycle Grid, including London's Quietways, can be found at <http://www.tfl.gov.uk/travel-information/improvements-and-projects/central-london-cycling-grid>

1. How did you find out about the proposals? (tick all which apply)

- I received a letter from Westminster City Council
- I attended the exhibition
- Word of mouth
- Social media
- Newspapers
- Websites
- Other

2. To what extent do you agree or disagree with the following statements?

	Strongly agree	Agree	Neither agree nor disagree	Disagree	Strongly disagree	Don't know
My enjoyment of Central London, and the City of Westminster, is affected by air quality	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
My enjoyment of Central London, and the City of Westminster, is affected by overcrowded public transport systems	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
My enjoyment of Central London, and the City of Westminster, is affected by too many road traffic collisions and casualties	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
My enjoyment of Central London, and the City of Westminster, is affected by traffic congestion	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

My enjoyment of Central London, and the City of Westminster, is affected by my ability to find a car parking space

3. To what extent do you agree or disagree with the following statements?

	Strongly agree	Agree	Neither agree nor disagree	Disagree	Strongly disagree	Don't know
More people cycling in Central London and the City of Westminster for everyday journeys can help to solve traffic congestion	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
More people cycling in Central London and the City of Westminster for everyday journeys can help to solve air quality	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
More people cycling in Central London and the City of Westminster for everyday journeys can help to solve overcrowded public transport systems	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
More people cycling in Central London and the City of Westminster for everyday journeys can help to solve road traffic collisions and casualties	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
More people cycling in Central London and the City of Westminster for everyday journeys can help to solve my ability to find a car parking space	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

4. In the area being consulted on, cycling conditions are currently...?

- Very good
- Fairly good
- Neither good nor poor
- Fairly poor
- Very poor

5. I would be more likely to cycle in central London if there was a network of easy to follow, quiet cycle routes.

- Strongly agree
- Agree
- No opinion
- Disagree
- Strongly disagree
- Don't know

6. To what extent do you support the proposals which are being consulted on at the moment along Hereford Road?

- Strongly support
- Tend to support
- Support some elements but not all
- Neither support nor oppose
- Tend to oppose
- Strongly oppose
- Don't know

7. To what extent do you support the proposals which are being consulted on at the moment along Talbot Road?

- Strongly support
- Tend to support
- Support some elements but not all
- Neither support nor oppose
- Tend to oppose
- Strongly oppose
- Don't know

8. If you have any particular concerns or comments about the scheme, please state them here:

.....

.....

.....

.....

.....

9. How often do you currently cycle?

- Everyday
- A few times a week
- About once a week
- A couple of times a month
- Once a month or less often
- Never

10. How often do you plan to cycle on the proposed Quietway cycle route?

- Everyday
- A few times a week
- About once a week
- A couple of times a month
- Once a month or less often
- Never

11. What age bracket do you fall into?

- Under 16
- 16 - 24
- 25 - 44
- 45 - 59
- 60+
- Prefer not to say

12. Are you?

- Male
- Female

13. Do you consider yourself to have a disability?

- Yes
- No
- Prefer not to say

14. Would you like to be kept informed on developments relating to this consultation and other initiatives relating to cycling and highway schemes that may impact on your local area?

- Yes
- No

15. If you replied 'Yes' to the previous question, please provide us with your name, email address and postcode so we may contact you. The information you provide will not be passed on to any other organisation.

Name:

Email address:

Postcode:

Thank you for completing this questionnaire. Please return the completed questionnaire to:
WSP Cycle Grid team
c/o FM Conway Ltd
25, Mandela Way
London
SE1 5SZ
Please return by 18 March 2016

Appendix F – Emails & Telephone Calls

Central London Cycle Grid - Quietway Bayswater Cycle Link

Date	Format	From	Comment (Redacted and Edited by consultation team)
02-Mar-16	Event	Local resident	Local resident wished to know if any parking would be lost in Hereford Road. Wishes additional cycle routes in the Parks.
02-Mar-16	Event	Local resident	Local resident stated that cyclists should be licensed. Concerned about behaviour of certain cyclists.
02-Mar-16	Event	Local residents	Two local residents, members of the Hereford Road Residents Association, raised the possibility of installing speed humps, as there is an issue of speeding vehicles. Also, raised the issue of narrow width of Hereford Street and congestion.
02-Mar-16	Event	Local resident	Local resident wished to know if any parking would be lost.
02-Mar-16	Event	SEBRA	Representative of SEBRA (South East Bayswater Residents Association) raised the following points: a/ wishes to keep the existing fingerpost cycle signs b/cycle-friendly gullies should not be installed in residents' bays.
02-Mar-16	Event	Local resident	Local resident is very supportive of cycling as a mode of transport, and wishes a lot more space to be allocated to cycling.
14-Mar-16	Event	Local residents	<p>My husband and I visited the Porchester Library yesterday to enquire about the proposed plans that would pass nearby our property.</p> <p>We were very impressed with the discussions your two advisors had with us, and found they were knowledgeable, and open to discussion. As cyclists and drivers we know the argument can be decisive but what we saw and what is on offer is a comprehensive approach to a tricky situation.</p>
18-Feb-16	Email	Local resident	<p>Overall, I think London has gone mad with cycling help. Yes - the big main roads, main roundabouts, bridges etc are a good idea. But quieter routes are an unnecessary addition as well as cost.</p> <p>The although short 1km route along Talbot Road, west of Ledbury Road affecting also Prince's Square, Leinster Square, Hereford Road is one such which is completely unnecessary. These are already quiet roads with very few cyclists and they are quite safe on them. If they have the use of the Westway cycle lanes they don't need extra facilities. The whole point I thought was to keep cyclists in safe lanes in the main roads, not progress them to quieter roads which they will know about anyway and where, if they ride properly, they can come to no harm.</p> <p>The junction of Hereford Road and Westbourne Grove (if I read correctly that the proposed route will cross there) is already a hazard for shopping pedestrians with fast approaching traffic and no lights at the junction. Sorting out this junction is more important than playing around with the proposed quiet roads. (During the plans for the adjacent new Sainsburys building it will be even more dangerous).</p> <p>Cyclists have been given so much priority, and with their consequent arrogance towards pedestrians the green man is no longer a safety zone. I have experienced myself when driving an ordinary car left at lights a cyclist riding fast and overtaking on the inside. Very frightening as there was nothing I could have done had he been unlucky. And it would have been my fault.....</p>
18-Feb-16	Email	Local resident	<p>I have looked at your website but do not really understand what the proposals mean. I can follow the route which I know quite well but what does it mean in terms of road markings and the effect on motorised vehicular traffic?</p> <p>I cannot see what the significance of a 'Quietway' is. Is it a route? Is it proposed to be marked?</p> <p>If you plan to add extra markings, this could be confusing to all road users. If the idea is to give something for cyclists to follow, this is not sensible. Anyone using a cycle should have an idea of where they want to ride to and should plan their route. Most people using say the Boris bikes, with the exception of a few foreign visitors, are mostly those cycling to work. In which case they know where they are going and are likely to follow their own planned route on a regular basis.</p> <p>My solution to the conflict between various road users is to cut the speed limit to 30 on say main roads such as Bayswater Road and 15 on all side roads. I do not like sleeping policemen and similar road calming but a firm control over speed should work.</p> <p>Let me know if you need any further comment, but of course I would love to have more specific details.</p>
01-Mar-16	Email	Local resident	<p>Your proposal to make the north part of Hereford Road W2 into part of the QUIETWAY in the cycle link is welcome but could be causing serious danger to cyclists because the road is currently used as a fast bypass for traffic to avoid the busy road / bus route and the two sets of traffic lights on Chepstow Road which is mostly subjected to single lane usage due to legal parking on both sides of the road.</p> <p>Hereford Road north of Westbourne Grove is residential and should be controlled by installing road humps to prevent excessive speed and may also be made one way only from Westbourne Grove, South to North.</p>
14-Mar-16	Email	London Fire Brigade	<p>The London Fire Brigade (LFB) supports the Mayor's Vision for Cycling and recognises the benefits which the proposed changes will bring to London and Londoners. The LFB also has a corporate travel plan, which includes measures to encourage our staff to choose more sustainable forms of transport for commuting and business travel, including cycling where possible.</p> <p>The LFB is happy to work with London Borough of Westminster on this area of work and is willing to provide support and guidance to the project through site visits and assist where it is practical and acceptable to do so. The Brigade's core principle will always be to provide the best possible service to the members of public we serve and to uphold the commitments made to the all communities that live and work in London Borough of Westminster.</p> <p>Full details of our standards can be found in our current London Safety Plan 5. This can be accessed on our website - http://www.london-fire.gov.uk/lsp5.asp</p> <p>LFB officers have visited the areas concerned and can confirm that the proposed work will have no effect on the LFB.</p>

Central London Cycle Grid - Quietway Bayswater Cycle Link

16-Mar-16	Email	Westminster Cycling Campaign	<p>Thank you for consulting us about this route. I am replying on behalf of Westminster Cycling Campaign, the local group of the London Cycling Campaign.</p> <p>This is an established cycle route, part of the London Cycle Network. It also benefits from comparatively low volumes of traffic. So it is a good candidate for a Quietway. However, a number of small problems along the route appear not to have been addressed, which lead us to qualify our support for the proposed Quietway.</p> <p>Hereford Road</p> <p>In some sections south of Westbourne Road, the free carriageway width is in the range 3.2-4 metres, which the London Cycle Design standards advise against. This is because motor vehicles might attempt to squeeze past cyclists, even though there is insufficient space to do so safely. However, there are a number of passing places along this end of the street.</p> <p>The crossing of Westbourne Grove can be quite difficult. It is also illogical that the 'keep clear' marking should be on the eastbound side of the road, where traffic is likely to be flowing, rather than westbound, where there is more likely to be a queue back from the nearby pedestrian crossing or the junction with Chepstow Road. Cyclists require more assistance at this point.</p> <p>When parking bays are fully occupied, there is a shortage of passing places on Hereford Road north of Westbourne Grove. This can cause difficulties for motor vehicles coming from opposite directions, though cyclists can often be delayed while motor vehicles sort themselves out.</p> <p>Rating: neither support nor oppose</p>
16-Mar-16	Email	Westminster Cycling Campaign (cont.)	<p>Talbot Road</p> <p>The arrangement at the junction of Talbot Road with Hereford Road has been successful in reducing traffic in Hereford Road while still allowing cycle access. Westminster would benefit from more schemes with similar intentions. However, the current design can be difficult to navigate on non-standard cycles, such as tandems and tricycles: a better design is called for.</p> <p>We are pleased to see that the guidance to provide advance stop lines at unsegregated signal-controlled junctions has been followed at the junction with Chepstow Road. However, these ASLs would benefit from lead-in lanes, space for which appears to be available. We also note that there is still no pedestrian phase at this junction and that cyclists will still conflict with turning vehicles.</p> <p>Rating: Support some elements but not all.</p> <p>We trust that the various issues we have raised can be addressed in the final design.</p>
04-Apr-16	Email	Local residents of Hereford Road	<p>I am writing with feedback from the meeting held on 22nd March between myself and some of the residents of Hereford Road.</p> <p>The meeting on the 22nd was attended by approximately twenty residents all wanting to air their views regarding the proposed cycle route down HR. Early on in the meeting I made it my business to communicate the bare facts, ie that a large increase in cyclists is not anticipated and that there will be no loss of parking bays. However, the residents remain very concerned about the effect of actively encouraging cyclists down a road that is perceived to be at tipping point with regards to general traffic chaos.</p> <p>The residents cited their two main issues on the street; 1. the road blocking that is often caused by trucks and large SUVs given the narrow street and, 2. the issue of speeding given that the road is used by many as a cut-through with vehicles speeding down the road once the road becomes clear. It became apparent at the meeting that, in the eyes of many residents, these issues make HR unsuitable as a bike route. In fact it was argued by some that it would be irresponsible of WCC/TCL to encourage bikes down a road that sees such regular road rage, speeding and blockages.</p> <p>However, the residents agreed that if measures were put in place to make HR safer and help the traffic flow more freely, then they would not seek to object to the proposed cycle route.</p> <p>After much debate, and having ruled out various other potential solutions eg a one way street, retractable bollards etc, on behalf of the residents of Hereford Road I would be grateful if you would consider the following proposal:</p> <ul style="list-style-type: none"> - the creation of a passing bay by extending the single yellow line outside number 51 (not losing a parking bay merely moving one a little further up the street) - the creation of several speed cushions (NB to be distinguished from speed bumps) along the length of the street. <p>I recently met with Sebra, who kindly advised of the plausibility of the above and I have separately sought advice on the credibility of speed cushions. It is my understanding that the cushions would not sit in the parking bays so should not make parking any more tricky than it is already.</p>

Appendix G – Data Analysis

Central London Cycle Grid

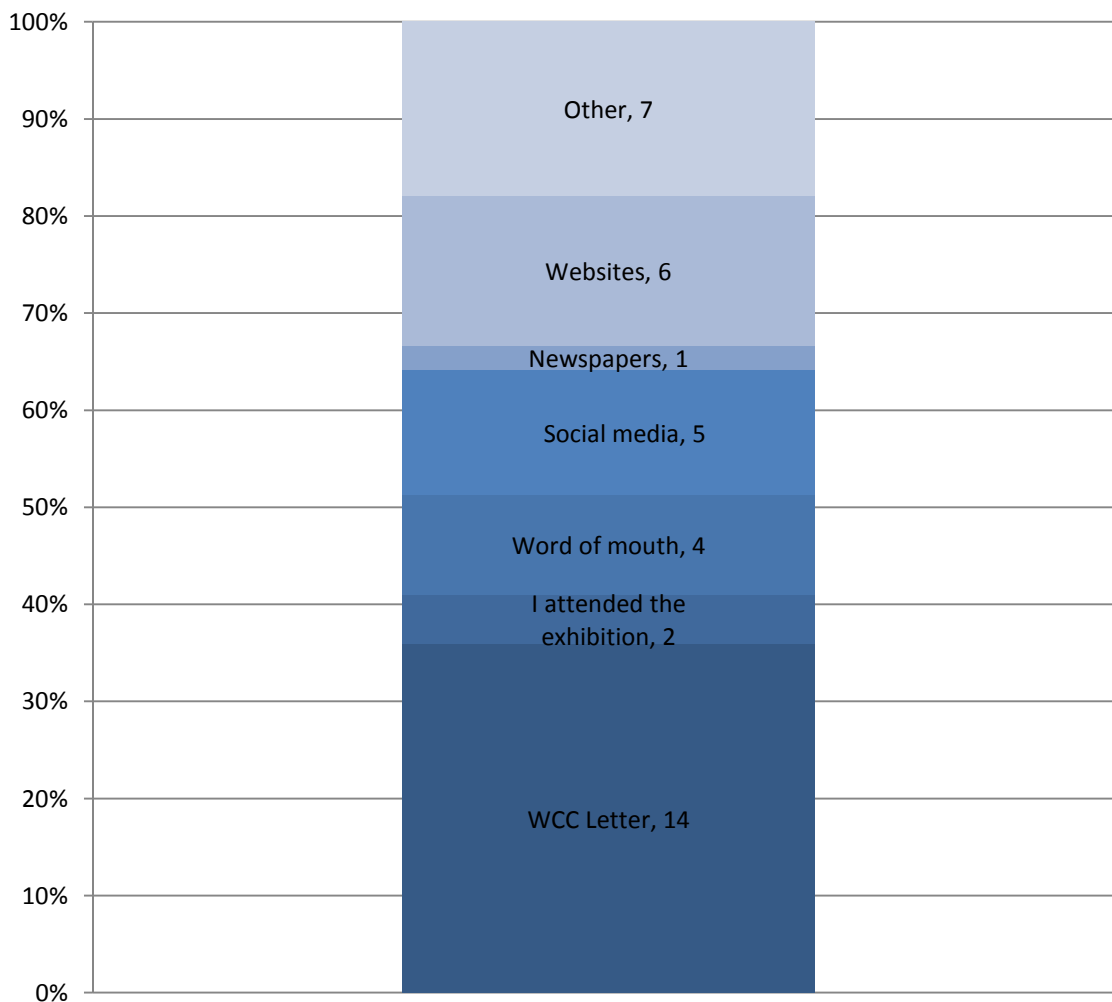
Quietway Cycle Route Link in Bayswater

Survey Responses during Public Consultation from 18 February 2016 to 18 March 2016

Total Usable Responses	37
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How did you find out about the proposals? (multiple choice)						
WCC Letter	I attended the exhibition	Word of mouth	Social media	Newspapers	Websites	Other
14	2	4	5	1	6	7

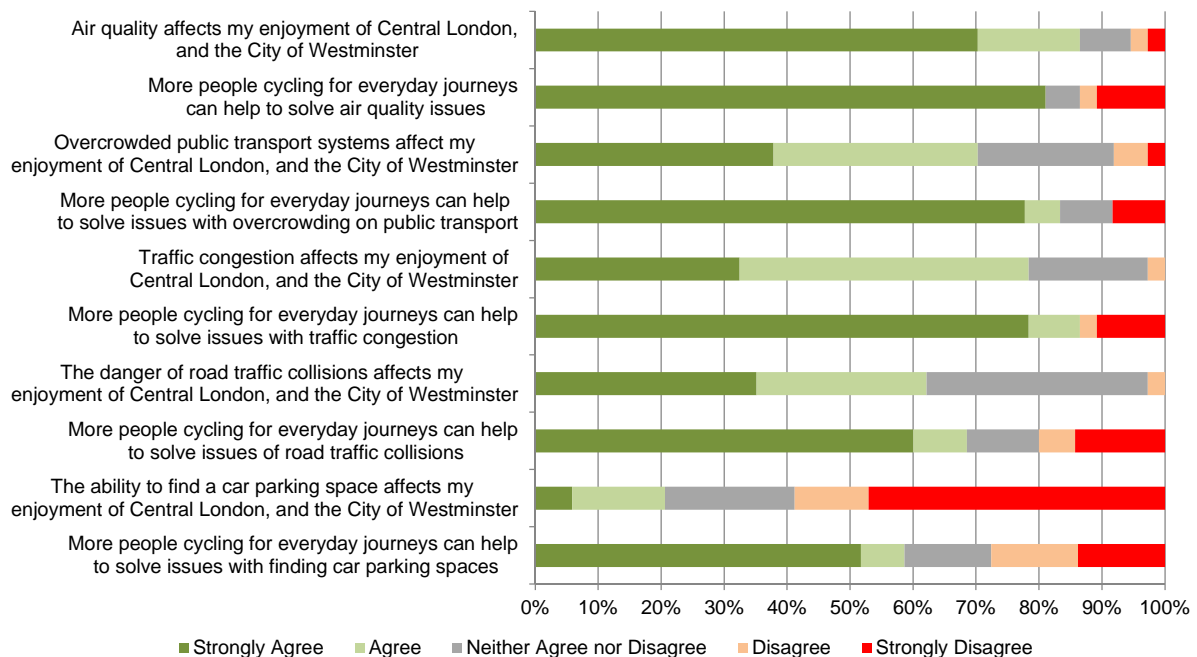
How did you find out about the proposals? (multiple choice)



What affects your enjoyment of Central London, and the City of Westminster? Could more cycling help solve these issues?

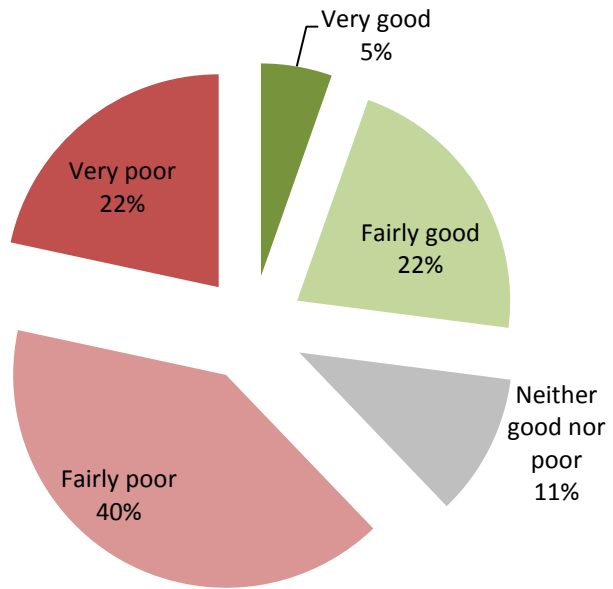
	More people cycling for everyday journeys can help to solve issues with finding car parking spaces	The ability to find a car parking space affects my enjoyment of Central London, and the City of Westminster	More people cycling for everyday journeys can help to solve issues of road traffic collisions	The danger of road traffic collisions affects my enjoyment of Central London, and the City of Westminster	More people cycling for everyday journeys can help to solve issues with traffic congestion	Traffic congestion affects my enjoyment of Central London, and the City of Westminster	More people cycling for everyday journeys can help to solve issues with overcrowding on public transport	Overcrowded public transport systems affect my enjoyment of Central London, and the City of Westminster	More people cycling for everyday journeys can help to solve air quality issues	Air quality affects my enjoyment of Central London, and the City of Westminster
Strongly Agree	15	2	21	13	29	12	28	14	30	26
Agree	2	5	3	10	3	17	2	12	0	6
Neither Agree nor Disagree	4	7	4	13	0	7	3	8	2	3
Disagree	4	4	2	1	1	1	0	2	1	1
Strongly Disagree	4	16	5	0	4	0	3	1	4	1

What affects your enjoyment of Central London, and the City of Westminster? Could more cycling help solve these issues?



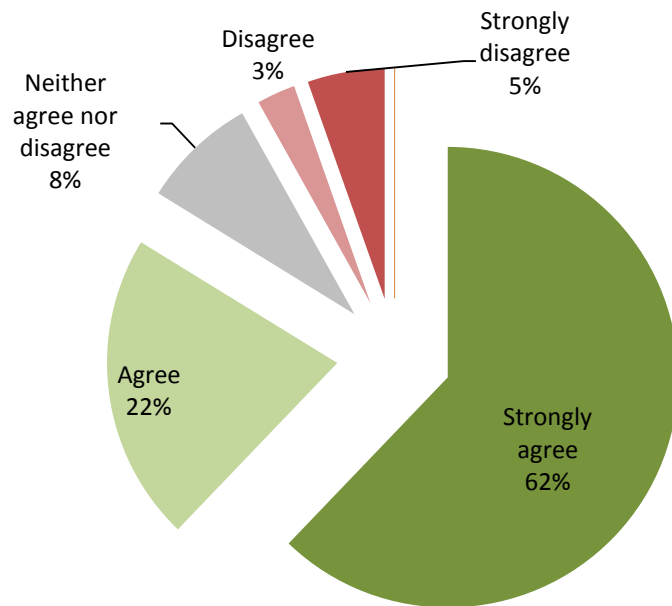
In the area being consulted on, cycling conditions are currently...?

Very good	2
Fairly good	8
Neither good nor poor	4
Fairly poor	15
Very poor	8



I would be more likely to cycle in central London if there was a network of easy to follow, quiet cycle routes.

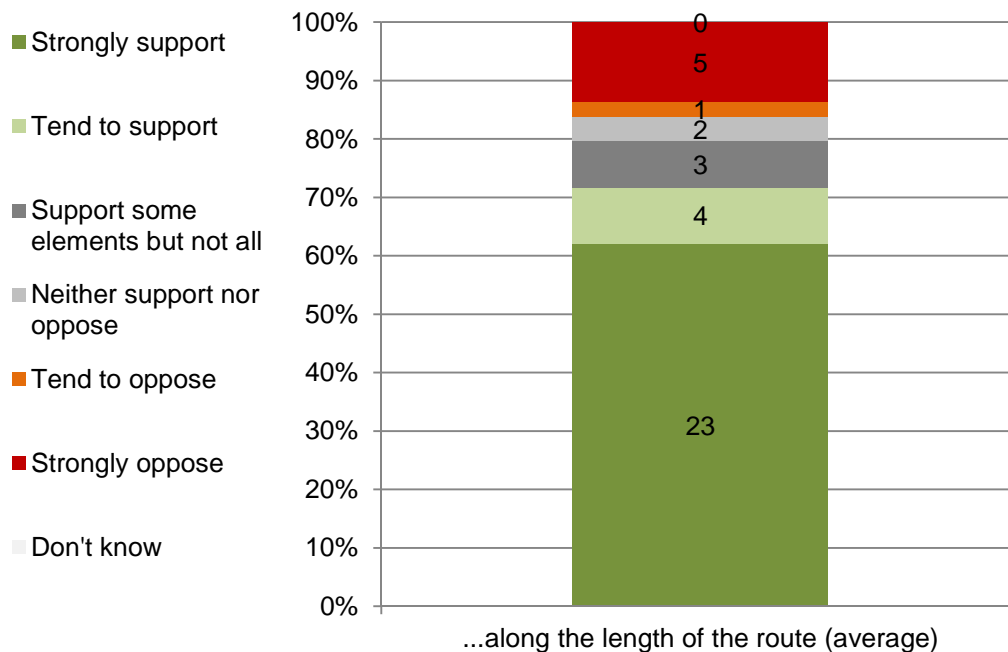
Strongly agree	23
Agree	8
Neither agree nor disagree	3
Disagree	1
Strongly disagree	2
Don't know	0



To what extent do you support the proposals which are being consulted on...?

	...along the length of the route (average)	...along Hereford Road?	...along Talbot Road?
Strongly support	23	23	23
Tend to support	4	3	4
Support some elements but not all	3	3	3
Neither support nor oppose	2	2	1
Tend to oppose	1	1	1
Strongly oppose	5	5	5
Don't know	0	0	0

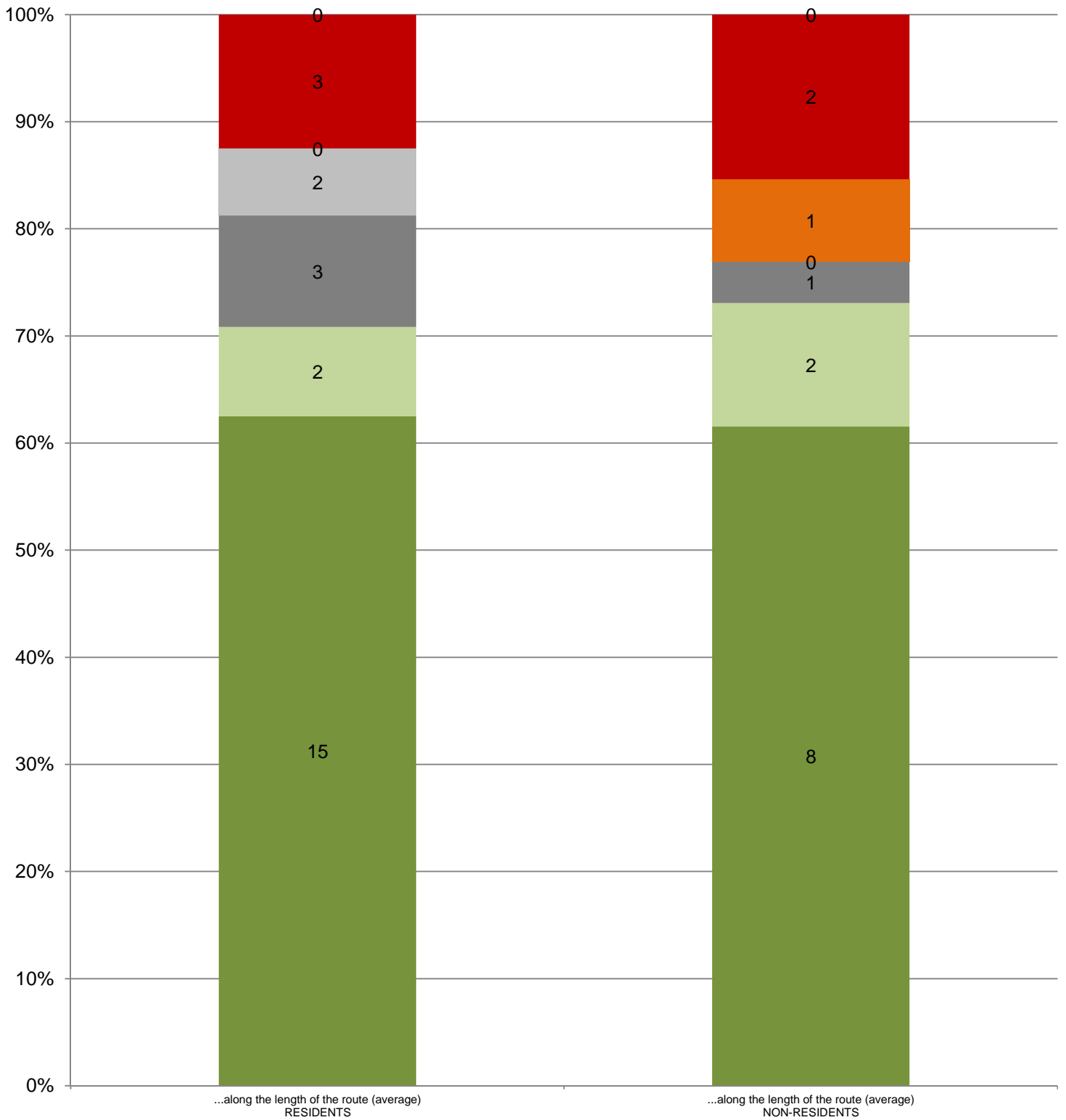
To what extent do you support the proposals which are being consulted on?



To what extent do you support the proposals which are being consulted on...? (Residents and Non-Residents)

	...along the length of the route (average) RESIDENTS	...along the length of the route (average) NON-RESIDENTS	...along Hereford Road ? RESIDENTS	...along Hereford Road ? NON-RESIDENTS	...along Talbot Road ? RESIDENTS	...along Talbot Road ? NON-RESIDENTS
Strongly support	15	8	15	8	15	8
Tend to support	2	2	2	1	2	2
Support some elements but not all	3	1	2	1	3	0
Neither support nor oppose	2	0	2	0	1	0
Tend to oppose	0	1	0	1	0	1
Strongly oppose	3	2	3	2	3	2
Don't know	0	0	0	0	0	0

**To what extent do you support the proposals which are being consulted on...?
(Residents and Non-Residents)**

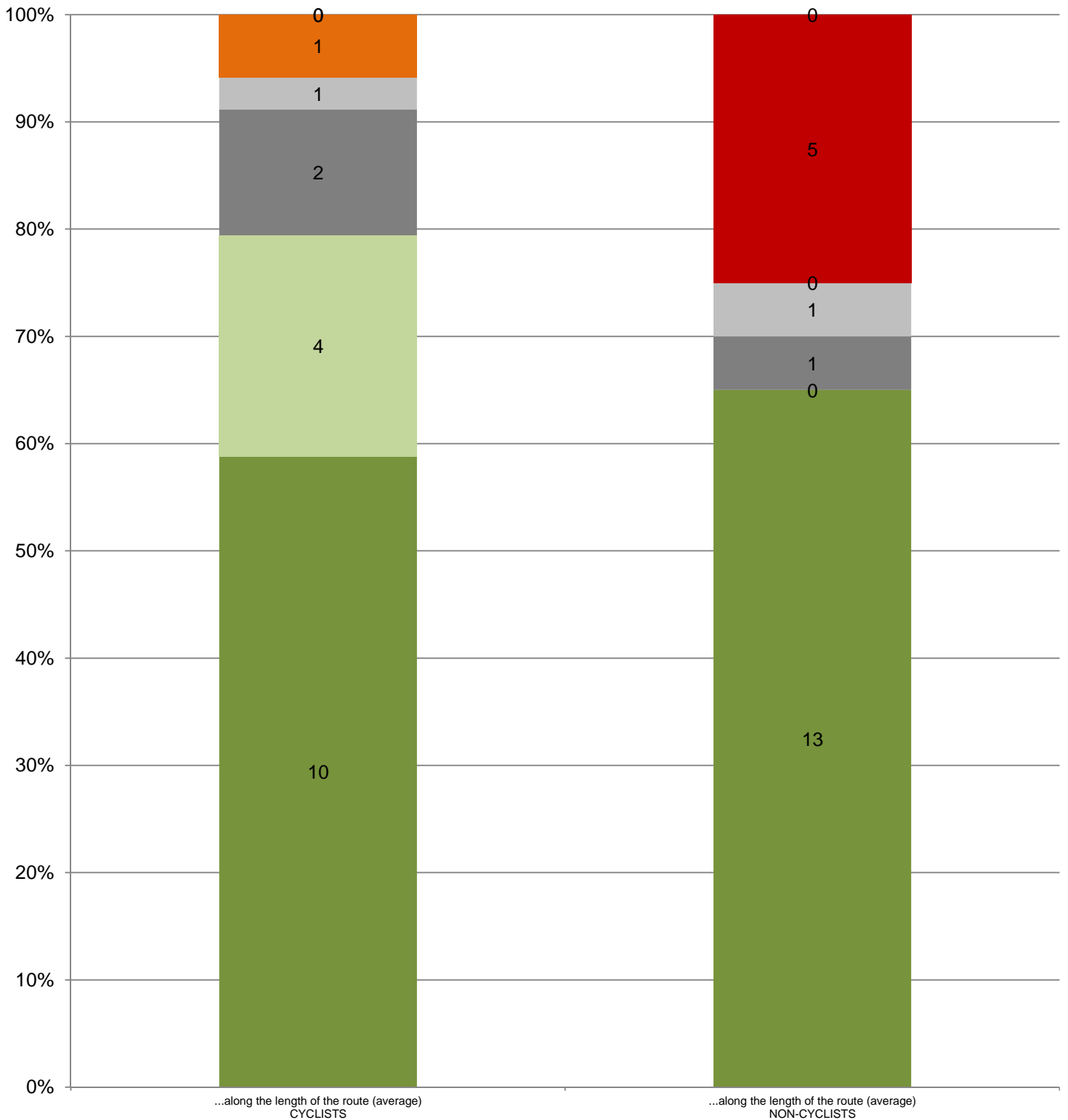


- Strongly support
- Support some elements but not all
- Tend to support
- Neither support nor oppose
- Tend to oppose
- Strongly oppose
- Don't know

To what extent do you support the proposals which are being consulted on...? (Cyclists and Non-Cyclists)

	...along the length of the route (average) CYCLISTS	...along the length of the route (average) NON-CYCLISTS	...along Hereford Road ? CYCLISTS	...along Hereford Road ? NON-CYCLISTS	...along Talbot Road ? CYCLISTS	...along Talbot Road ? NON-CYCLISTS
Strongly support	10	13	10	13	10	13
Tend to support	4	0	3	0	4	0
Support some elements but not all	2	1	2	1	2	1
Neither support nor oppose	1	1	1	1	0	1
Tend to oppose	1	0	1	0	1	0
Strongly oppose	0	5	0	5	0	5
Don't know	0	0	0	0	0	0

**To what extent do you support the proposals which are being consulted on...?
(Cyclists and Non-Cyclists)**



- Strongly support
- Support some elements but not all
- Tend to oppose
- Don't know
- Tend to support
- Strongly oppose

Comments received in the online survey

General comment about the Quietway proposals

To what extent do you support the proposals along Hereford Road ?

Strongly support

I cycle this route every day to work - it is already fairly nice to cycle down, but these proposals will make it even better!

Sadly I feel that slightly too little is being done in these areas. Such frequent wayfinding symbols are not necessary, and there should be traffic calming measures instead.

Still would like to see an East West cycle track inside Kensington Gardens, parallel to Bayswater Road. Also need Santander bike availability in the St Stephens area of W2, where there is currently none.

Tend to support

St Petersburg Place would be better than Osnaburgh. It's broader in front of the church and next street to south affording easier respite, and better connection with public buildings, the 3 churches there including synagogue and Greek orthodox.

Support some elements but not all

I would like Hereford Road to remain 2 way.

It is insane to have shared use between cyclists and pedestrians. 13 stone mules cycling at high speed are a danger to children and other vulnerable users. Please, please, please: no shared use.

TFL should take a more holistic look at potential cycle routes, focusing on displacing cycle journeys away from the main road network (e.g. Bayswater Rd, Edgware Rd) onto quieter streets, improving the relative accessibility and journey times by introducing a combination of :

- contraflow no segregated cycle lanes on one-way Traffic streets and "cells"
- more cycle-friendly minor road crossing points, avoiding major road intersections
- more cycle route solutions for crossing heavily congested Central London areas : e.g. Soho, Holborn, Islington, Farringdon

Neither support nor oppose

This is the response of Westminster Cycling Campaign:

This is an established cycle route, part of the London Cycle Network. It also benefits from comparatively low volumes of traffic. So it is a good candidate for a Quietway. However, a number of small problems along the route appear not to have been addressed, which lead us to qualify our support for the proposed Quietway.

Hereford Road

In some sections south of Westbourne Road, the free carriageway width is in the range 3.2-4 metres, which the London Cycle Design standards advise against. This is because motor vehicles might attempt to squeeze past cyclists, even though there is insufficient space to do so safely. However, there are a number of passing places along this end of the street.

The crossing of Westbourne Grove can be quite difficult. It is also illogical that the 'keep clear' marking should be on the eastbound side of the ride, where traffic is likely to be flowing, rather than westbound, where there is more likely to be a queue back from the nearby pedestrian crossing or the junction with Chepstow Road. Cyclists require more assistance at this point.

When parking bays are fully occupied, there is a shortage of passing places on Hereford Road north of Westbourne Grove. This can cause difficulties for motor vehicles coming from opposite directions, though cyclists can also be delayed while motor vehicles sort themselves out.

 Talbot Road

The arrangement at the junction of Talbot Road with Hereford Road has been successful in reducing traffic in Hereford Road while still allowing cycle access. Westminster would benefit from more schemes with similar intentions. However, the current design can be difficult to navigate on non-standard cycles, such as tandems and tricycles: a better design is called for.

We are pleased to see that the guidance to provide advance stop lines at unsegregated signal-controlled junctions has been followed at the junction with Chepstow Road. However, these ASLs would benefit from lead-in lanes, space for which appears to be available. We also note that there is still no pedestrian phase at this junction and that cyclists will still conflict with turning vehicles.

 Tend to oppose

This response is made on behalf of the London Cycling Campaign, the capital's leading cycling organisation with more than 12,000 members and 40,000 supporters. We welcome the opportunity to comment on these plans and our response was developed with input from the co-chairs of our Infrastructure Review Group and is in support of the response of Westminster Cycling Campaign, our local group.

We welcome the theory of Quietways targeting less confident cyclists who want to use low-traffic routes, while also providing capacity and maximum route choice for existing cyclists. We also welcome the Mayor's vision for Quietways that are direct, designed as whole routes, segregated from motor traffic where they briefly join busy roads and make use of "filtered permeability" that restricts through motor traffic etc.

Sadly, our assessment based on the first routes to reach public consultation is that Quietways thus far fail to fulfil these ambitions to the degree needed to genuinely boost cycling numbers. In this link, we again see some minor positives, but the big issues largely not tackled – and thus we cannot support this scheme without further improvements to it. We would like to highlight the following issues as most pressing:

1. There are many sections of the scheme that see carriageway widths in the "critical fail" boundaries of 3.2-4m according to TfL's Cycling Level of Service (CLoS) document. This includes Talbot Road, Prince's Square and Hereford Road.
2. The current design for the crossing of Westbourne Grove is in no way suitable for anything called a "Quietway". Westbourne Grove is a busy road with a history of collisions with cyclists. Yet there is nothing proposed that isn't already there. Cyclists will be expected to dart or weave out into heavy or queuing traffic. This junction needs looking at again as a matter of urgency. One option would be to introduce modal filters on the side roads, and relocate the existing pedestrian crossing to the stopped up side roads, making it a "tiger" crossing. Failing that, some other method of ensuring safe, comfortable and convenient crossing at this location is vital.

3. The main junction on Talbot Road at Chepstow Road is also busy, including with double-decker buses, and requires more than just ASLs for appropriate protection and to eliminate hook risks, while allowing safe turns in all directions for cyclists.

4. There is a lack of appropriate motor vehicle speed control throughout - particularly on Hereford Road and on Talbot Road. Sinusoidal speed humps and/or raised tables are required.

5. The existing facility at the junction of Talbot Road and Hereford Road is proposed to be retained, but does not appear to be wide enough for non-standard bikes (cargo, mobility etc.), nor does it provide an easy and comfortable turn-in and wait location. Please redesign this facility to improve it.

In general, the London Cycling Campaign want, as a condition of funding, all highway development designed to London Cycling Design Standards (LCDS), with a Cycling Level of Service (CLoS) rating of 70 or above, with all "Critical Fails" eliminated.

 Strongly oppose

Cyclists on the Bayswater Road cause huge traffic problems. The worst decision ever made was to put hire bikes in the parks - most people who use them are inexperienced and wear no protective gear. In my opinion all road users including cyclists should be insured at least against third party claims. Cyclists are just as likely as anyone else to cause an accident but are generally not liable for their actions.

.....
During peak hours there are too many cyclists and many of them are dangerous. I have been knocked down by another cyclist! I m now too scared to use a bike again.
On the proposed route there are lots of blocks of flats. These residents have cars. We have already lost many parking places to the hire bike racks. are more to be eliminated? If you look at the picture on the opening screen you will see cyclists massed in front of cars. Bikes are the slowest form of transport but they are in the front, holding up everyone else! Instead of easing the flow of traffic, bikes just slow down the whole traffic system of public & private transport. It works in Holland where the cyclists' attitude to travel is conciliatory and the roads adequately laid out, but here it is horrendous for vehicles, the cyclists themselves and pedestrians.

.....
The current cycle lane in Talbot Road is poorly maintained and never used. As it is narrow and paved either side, litter collects within the cycle lane and is a constant source of filth

.....
Wayfinding symbols are a waste of money. People know where to go, the reason they do not cycle is that it is not safe enough for them to do so. This proposal does nothing to improve cycling safety.

.....

